



TARGA

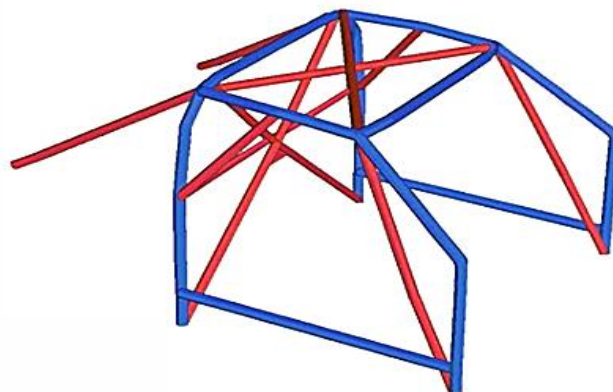
3/31 Davey Street,
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7000

2023 TARGA VEHICLE CHANGES AND COMPETITIONS GUIDE

VEHICLE CHANGES GUIDE

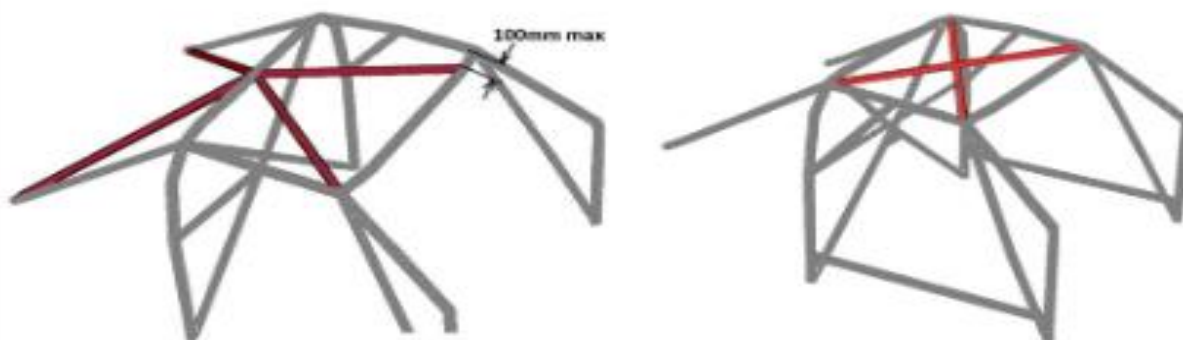
Safety Cages

All competition vehicles, except TSD Trophy, must be fitted with a full specification safety cage, as shown in the various diagrams and instructions below.



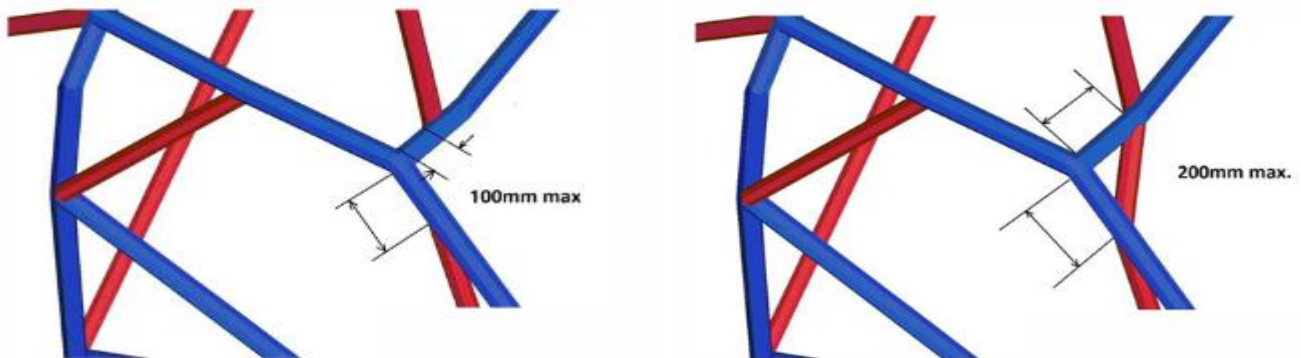
Roof Reinforcement

The safety cage must contain a reinforcement to brace the roof area above the occupants. This may take the form of a "V" or "cross". At the forward end, each roof reinforcement member must be attached to the header bar either within 100mm of the junction of the front leg or within 200mm if a reinforcement is fitted.



A-Pillar Reinforcement

The purpose of the A-Pillar reinforcement is to triangulate the forward edge of the cage. At its lower end, it must be mounted to either the same mounting foot or reinforcement plate as the front leg, or to a separate mounting foot or chassis reinforcement to the same specification. At the upper end it must be attached to the front leg either within 100mm of the junction of the header bar, or within 200mm if a reinforcement is fitted.

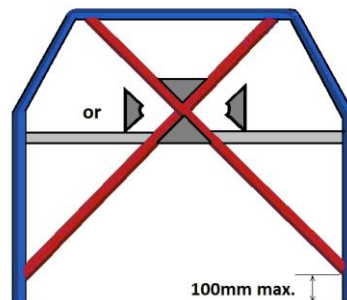


Main Hoop Cross

The main hoop shall be reinforced by at least two diagonal members forming a cross. The junction between the two members shall be reinforced by a pair of opposed gussets. The diagonal braces must terminate within 100mm of the mountings of the main hoop at the bottom and the junction of the rear braces at the top.

Safety Harness Bar

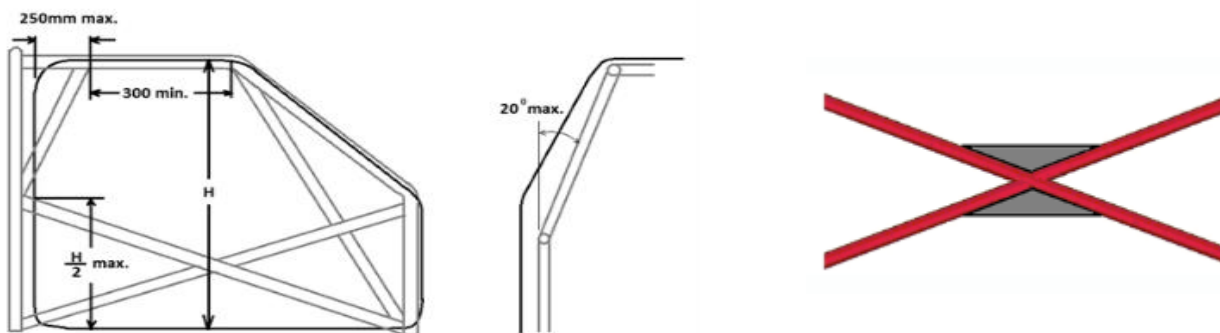
- A safety harness bar, where fitted, must extend the full width of the main hoop.
- Where the harness bar is intersected by a diagonal member in the plane of the main hoop, each section of the harness bar shall be co-linear.
- Threaded ferrules (7/16"-20 or M11 x 1.25) may be added to the harness bar. Where fitted these shall extend through the tube and be fully welded.
- Guides may be added to the harness bar to limit lateral movement of the harness where the harness is attached by wrapping.
- Where foreseen by the seat manufacturer, the seat backrest may be attached to the harness bar.



Anti-Intrusion Bars and Windscreen Pillar Reinforcement

The fitting of two anti-intrusion door bars is required. It is preferred that each such bar be one continuous tube. In cases where the two bars intersect, resulting in a total vertical cross section less than that of the two separate bars, the junction must be reinforced by a pair of opposed gussets fitted to the top and bottom.

Each anti-intrusion bar shall extend from the main hoop to the front leg and be made of a single tube. If practical, it should be bent out towards the exterior of the vehicle, but otherwise straight. Each anti-intrusion bar must be as high as possible, but not above a point halfway up the door opening at either end.



Side Protection Foam

Side protection foam is required in all full competition vehicles and highly recommended in all speed limited vehicles. Doors may be original equipment or aftermarket equipment of free material, providing it retains the original shape. Energy absorbing safety foam must be fitted to each front door of the vehicle and for two door vehicles, also fitted in the bodywork immediately rearward of the door aperture. The minimum volume of foam in each application must be 60 litres, using an internationally recognised motorsport sanctioning body approved foam.

For the purpose of fitting foam it is permitted to

- i. Modify the interior part of the door or the aperture for the fitment of foam, provided that the structural and safety integrity of the door is not compromised. This may include the removal of window mechanisms and other door hardware.
- ii. Modify the internal door opening mechanism. It is not permitted to modify the original door latch mechanism.
- iii. Additional foam may be fitted in the space between the inner door panel and each seat.
- iv. Where the winged race seat is fitted, additional foam may be fitted to fill the void that exists between the side of the seat head protection and the side window or B-Pillar. In this case the foam shall be fixed to the seat side with Velcro® or similar, hook and loop fastener.
- v. Each door or aperture fitted with foam shall have a trim panel fitted that consists of a minimum of six ply carbon fibre and/or kevlar reinforced composite material.
- vi. It is the responsibility of the competitor to provide proof that the foam used is compliant with TARGA requirements.
- vii. It is permitted to replace each side window on the basis of the following;
 - (A) The original shape of the original window is retained;
 - (B) The um thickness of the polycarbonate replacement is 3.8mm;
 - (C) Can be removed both internally and externally without tools in an emergency situation;
 - (D) It is permitted to fit a closable aperture, of the same polycarbonate material, using a sliding mechanism, within the replacement window.

Winged Race Seats

Winged race seats are compulsory for driver and navigator/co-driver in all competition vehicles, except those competing in TSD Trophy. All seats must comply with a current internationally recognised motorsport sanctioning body homologated standard. It is also highly recommended that current internationally recognised motorsport sanctioning body homologated seat mounts are used to work effectively with the fitted door foam.

Minimum Head Space

No part of a crew member's helmet can be less than 50mm below a line drawn between the highest point of the main safety cage hoop and the highest part of the front roll bar or less than 50mm from any roof bar lateral member ignoring any foam padding.

Fire Extinguishers

All competition vehicles must carry a minimum of 4kg of fire suppression material either via Australian Standard fire extinguishers or via the fitment of an approved plumbed-in fire suppression system. Extinguishers must be fitted in a secure location that is easily and quickly accessible by driver and navigator/co-driver.

Anti-Lock Braking Systems (ABS)

ABS may be retrofitted to all competition vehicles, which don't have ABS fitted from the manufacturer. Vehicles with ABS fitted from the manufacturer may change this ABS system.

Tyres

Tyres must be legal for road use in Australia. A maximum of ten tyres are allowed for each event. All tyres must have a minimum tread depth of 1.5mm at any point on the tread normally in contact with the road other than at tread depth indicators, at all times.

Video Cameras

All competition vehicles must be fitted with a forward-facing on-board camera, which can store vision data for a minimum of 24 hours.

COMPETITION CATEGORIES GUIDE

INTRODUCTION

As part of our hard reset for a bright future, we have taken on board a lot of input into our competition structure for future events. With the passing of time and younger generations coming into TARGA with their own late 20th century 'classic' vehicles from when they were young, we felt it was time to not only protect the traditional classic vehicles from an invasion of later model vehicles, but also provide suitable competition for those with vehicles that are between 15 and 35 years old without the fear of also being invaded from the world's most modern vehicles. The Rookie Rallye will also be re-introduced to TARGA Tasmania to provide a suitable training pathway for new competitors.

A choice of seven competitions, plus the Rookie Rallye, are available to ensure large quality competitions are retained for all TARGA competitors to enjoy. Full and speed-limited competitions will be directly aligned for the first time to provide a clear pathway between the two levels. Categories and classes within each competition will remain as they have been in previous years and the full TARGA Tasmania plate awards system is also retained.

Please see below the updated list of competitions available at each TARGA event in the future.

TSD TROPHY

This all vehicles regularity (average speed) based competition is speed limited to 130km/h and is reserved for vehicles manufactured from 1 January 1900 up to the first day of competition. Vehicles must have been commercially available as a road registerable vehicle when manufactured, or be a vehicle approved by TARGA. Vehicles must be compliant for normal road registration and road usage including any modifications that have been made to the vehicle.

CLASSIC TROPHY- HANDICAP (NEW NAME)

This true classic vehicle competition is speed limited to 130km/h and is reserved for 2WD vehicles only, manufactured from 1 January 1900 up to 31 December 1985, plus approved run/ons up to 31 December 1989, as shown on the vehicle ID plate. Vehicles must have been commercially available as a road registerable vehicle when manufactured, or be a vehicle approved by TARGA. Potential performance will be managed by the application of a Standard Specification (SS), Limited Modified Specification (LMS) or Modified Specification (MS) based handicap for each vehicle to determine the winners.

EARLY MODERN TROPHY (NEW COMPETITION)

This late 20th and early 21st century vehicle competition is speed limited to 130km/h and is reserved for vehicles, manufactured from 1 January 1986 up to 31 December 2006, plus approved run/ons up to 31 December 2009, as shown on the vehicle ID plate. Four-Wheel Drive vehicles built before 1 January 1986 are also eligible. Vehicles must have been commercially available as a road registerable vehicle when manufactured, or be a vehicle approved by TARGA.

MODERN TROPHY (NEW NAME)

This modern vehicle competition is speed limited to 130km/h and is reserved for vehicles manufactured from 1 January 2007 up to the first day of competition. Vehicles must have been commercially available as a road registerable vehicle when manufactured, or be a vehicle approved by TARGA.

ROOKIE RALLYE (TARGA TASMANIA ONLY)

This all vehicles competition is speed limited to 130km/h and is reserved for vehicles manufactured from 1 January 1900 up to the first day of competition and competitors who have never previously competed in TARGA Tasmania. Vehicles must have been commercially available as a road registerable vehicle when manufactured, or be a vehicle approved by TARGA. The event will be conducted over days 1 to 3 of TARGA Tasmania.

CLASSIC- HANDICAP

This true classic vehicle competition is reserved for 2WD vehicles only, manufactured from 1 January 1900 up to 31 December 1985, plus approved run/ons up to 31 December 1989, as shown on the vehicle ID plate. Vehicles must have been commercially available as a road registerable vehicle when manufactured, or be a vehicle approved by TARGA. Potential performance will be managed by the application of a Standard Specification (SS), Limited Modified Specification (LMS) or Modified Specification (MS) based handicap for each vehicle to determine the winners.

Please note- The Classic GT competition has been merged back into this competition with a Modified Specification re-introduced to accommodate vehicles previously entered in Classic GT.

EARLY MODERN

This late 20th and early 21st century vehicle competition is reserved for vehicles, manufactured from 1 January 1986 up to 31 December 2006, plus approved run/ons up to 31 December 2009, as shown on the vehicle ID plate. Four-Wheel Drive vehicles built before 1 January 1986 are also eligible. Vehicles must have been commercially available as a road registerable vehicle when manufactured, or be a vehicle approved by TARGA.

MODERN (NEW NAME)

This modern vehicle competition is reserved for vehicles manufactured from 1 January 2007 up to the first day of competition. Vehicles must be (or have been) commercially available as a road registerable vehicle when manufactured, or be a vehicle approved by TARGA. The 1st, 2nd and 3rd placegetters in this competition will also be recognised as the Overall winners of the TARGA event, regardless of total times scored by vehicles in other competitions, due to the additional technical freedoms allowed in some other competitions and the possibility of having travelled a different course.